

PLATE No. 1

CONSTRUCTION NOTES:

GRANULAR BASE:

MIN 6" OF CLASS 5 AGGREGATE BASE OR MATCH EXISTING SECTION

BITUMINOUS:

SHALL BE PLACED A MINIMUM OF 6" THICK ON MIN 6" OF CLASS 5 AGGREGATE BASE. MATCH EXISTING BITUMINOUS DEPTH IF GREATER THAN 6"

REINFORCEMENT:

ALL REBAR SHALL BE EPOXY COATED WITH NO VISIBLE DINGS, SCRATCHES OR OTHER EXPOSED METAL. ALL REBAR SHALL BE SUPPORTED BY CHAIRS AT 4'-0" O.C. MAXIMUM. DOWELS CAGES MAY BE REQUIRED ON NEW CONCRETE SECTIONS AS DESIGNATED BY THE ENGINEER.

CONCRETE:

SHALL BE A MINIMUM OF 7" THICK. MATCH EXISTING DEPTH IF GREATER THAN 7". REINFORCE W/#5 BARS @ 24" O.C. AND #5 x 18" DOWELS AT 24" O.C. OR AS APPROVED. DOWELS WILL BE DRILLED 9" INTO EXISTING PAVEMENT THROUGH PRE-DRILLED HOLES NO GREATER THAN 1/8" LARGER DIAMETER THAN THE REBAR, ANCHOR WITH EPOXY OR NON-SHRINK GROUT. ADDITIONAL REINFORCEMENT MAY BE NECESSARY DEPENDING ON THE LOCATION AND SHAPE OF THE PATCH. THE CONTRACTOR SHALL BE REQUIRED TO MATCH EXISTING TRANSVERSE AND LONGITUDINAL JOINTS.

BITUMINOUS OVERLAID CONCRETE:

PLACE 7" OF CONCRETE AND DOWEL INTO EXISTING PAVEMENT AND MATCH JOINTS AS NOTED ABOVE. PLACE BITUMINOUS TO MATCH EXISTING.

THE FOLLOWING STREETS AND AVENUES REQUIRE CDF BACKFILL:

STREETS; 4TH, 5TH, 8TH, 11TH, 14TH, 17TH, 20TH, 21ST, 26TH STREET (24TH TO 28TH AVE), 28TH NORTH, 28TH SOUTH (FROM VILLAGE GREEN BLVD), 34TH AND 40TH
 AVENUES; 1ST NORTH, CENTER, MAIN, SE MAIN, 4TH SOUTH, 7TH SOUTH, 12TH SOUTH, 20TH SOUTH, 24TH SOUTH, 30TH SOUTH, 40TH SOUTH, 46TH SOUTH, 50TH SOUTH, VILLAGE GREEN BOULEVARD, RIVERHAVEN ROAD, ALL OF 15TH AVENUE NORTH, AND 8TH AVENUE NORTH (EAST OF HWY 75).

*PAVE PREP SHALL BE INSTALLED ACCORDING TO THE CITY OF MOORHEAD SPECIFICATIONS

NOTE:

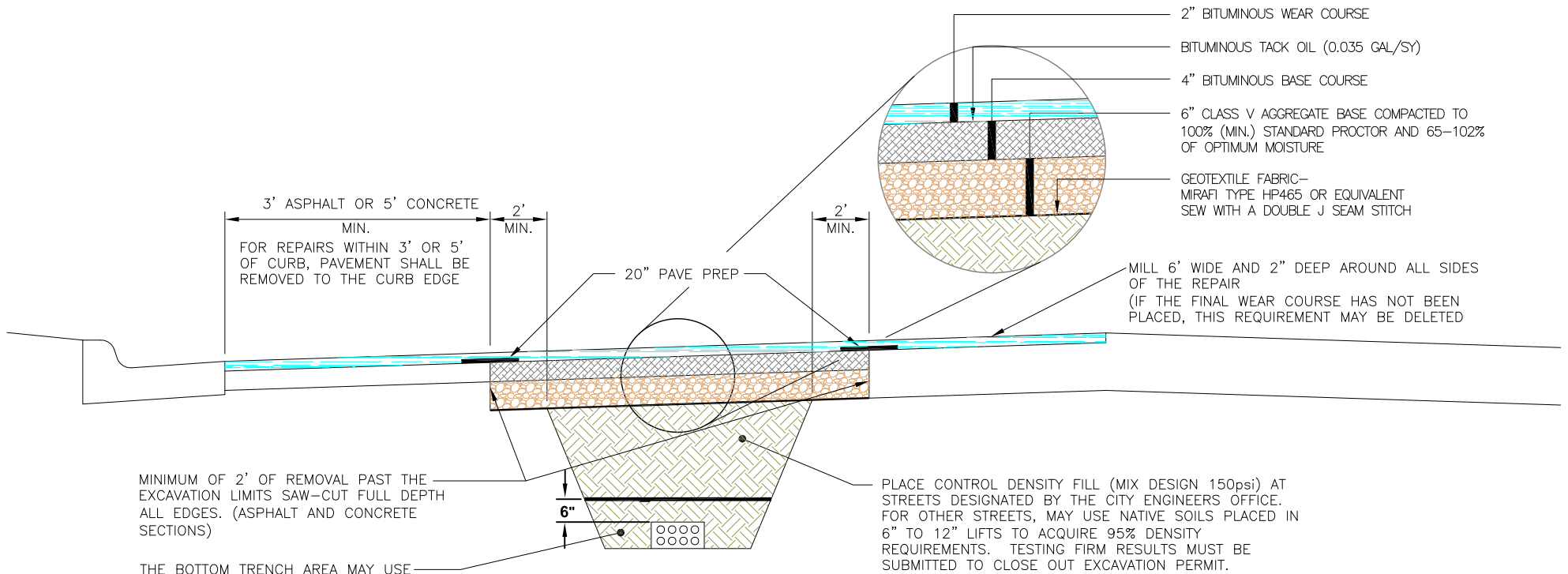
TEMPORARY COLD WEATHER PATCH SHALL CONSIST OF 4" OF COLD-MIX FOR COLLECTOR STREETS AND 2" OF COLD-MIX FOR LOCAL STREETS TO BE MAINTAINED UNTIL PERMANENT PAVEMENT IS INSTALLED

CONCRETE PAVEMENT REPAIRS WITHIN 5' OF EXISTING JOINT (LONGITUDINAL OR TRANSVERSE) MUST BE MADE TO THE JOINT

ALL REBAR USED IN STREET AND CURB REPAIR WILL BE EPOXY COATED IN ACCORDANCE WITH MNDOT SPECIFICATION 3301

CONCRETE CURBING SHALL BE REMOVED TO THE NEXT JOINT IF LESS THAN 3' TO REMOVAL AREA OR THE REMOVAL SHALL BE TO THE OTHER SIDE OF A BROKEN CURB SECTION AS DETERMINED BY THE ENGINEER OR HIS DESIGNATED REPRESENTATIVE

DRIVEWAY REMOVALS WITHIN THE RIGHT-OF-WAY SHALL EXTEND TO THE BACK OF CURB OR THE SIDEWALK CROSSING PLATE IF LESS THAN 3' OF DRIVEWAY REMAINS ADJACENT TO EXCAVATION AREAS



MINIMUM STANDARDS FOR NEWER STREET REPAIR

LESS THAN 10 YEARS SINCE CONSTRUCTION OR REHABILITATION
 NOT TO SCALE

PLATE No. 2

NOTE:

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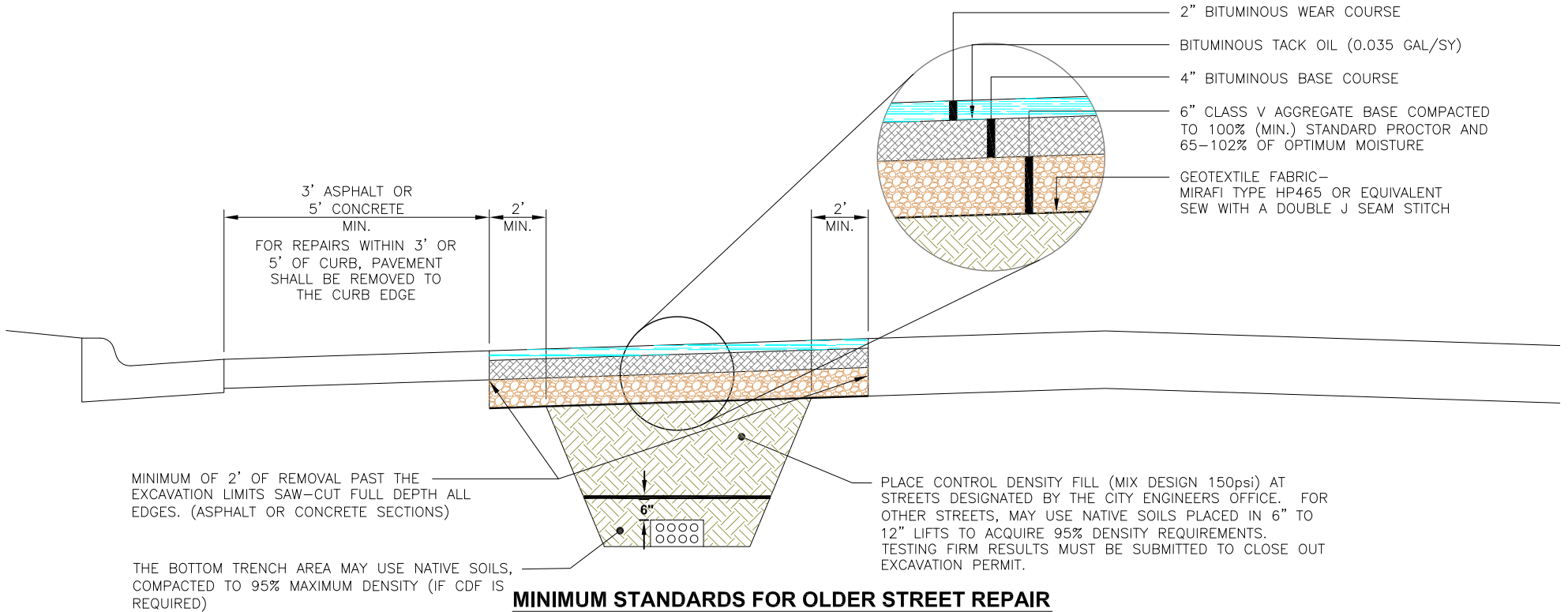
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MINIMUM STANDARDS FOR OLDER STREET REPAIR

GREATER THAN 10 YEARS SINCE CONSTRUCTION OR REHABILITATION
 NOT TO SCALE

PLATE No. 3 (CIP AREAS)

NOTE:

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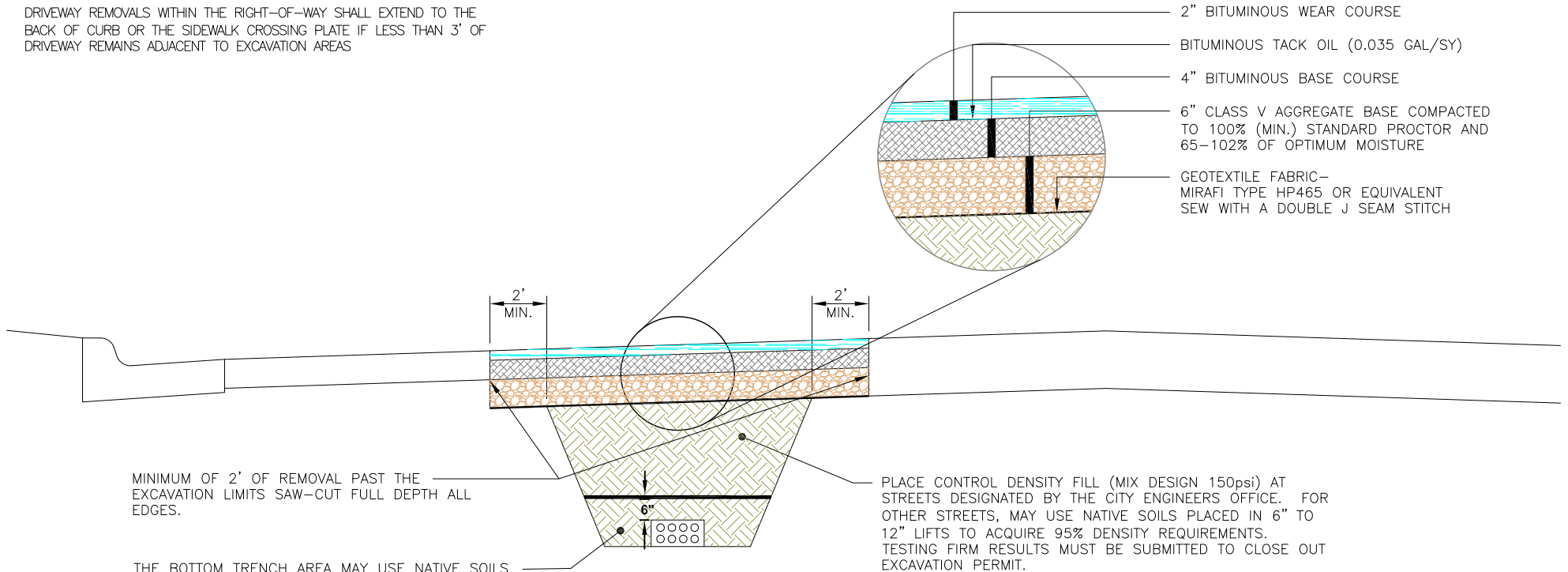
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MINIMUM OF 2' OF REMOVAL PAST THE EXCAVATION LIMITS SAW-CUT FULL DEPTH ALL EDGES.

THE BOTTOM TRENCH AREA MAY USE NATIVE SOILS, COMPACTED TO 95% MAXIMUM DENSITY (IF CDF IS REQUIRED)

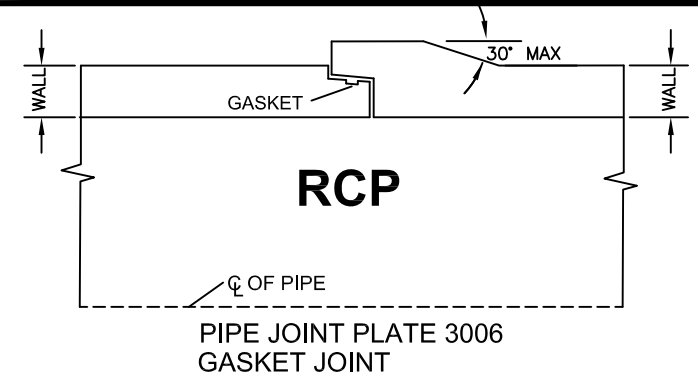
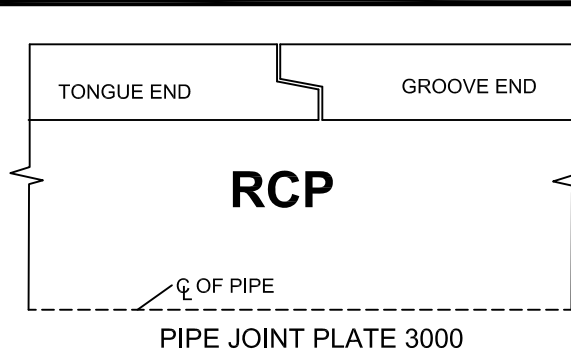
PLACE CONTROL DENSITY FILL (MIX DESIGN 150psi) AT STREETS DESIGNATED BY THE CITY ENGINEERS OFFICE. FOR OTHER STREETS, MAY USE NATIVE SOILS PLACED IN 6" TO 12" LIFTS TO ACQUIRE 95% DENSITY REQUIREMENTS. TESTING FIRM RESULTS MUST BE SUBMITTED TO CLOSE OUT EXCAVATION PERMIT.

MINIMUM STANDARDS FOR STREET REPAIR

STREETS IN THE 5 YEAR C.I.P.
 NOT TO SCALE

STORM SEWER REPAIRS

(WITHIN CITY RIGHT-OF-WAY)



NOTES:

1. REINFORCEMENT NOT SHOWN
2. THERE SHALL BE 18" OPENINGS IN CONCRETE BARREL
3. TYPE "D" CASTING ASSEMBLY TO BE USED IN B-624 CURB & GUTTER
4. TYPE "C" CASTING ASSEMBLY TO BE USED IN TYPE "C" MOUNTABLE CURB & GUTTER, REAR YARD INLET AND STREET DRAINAGE SYSTEM

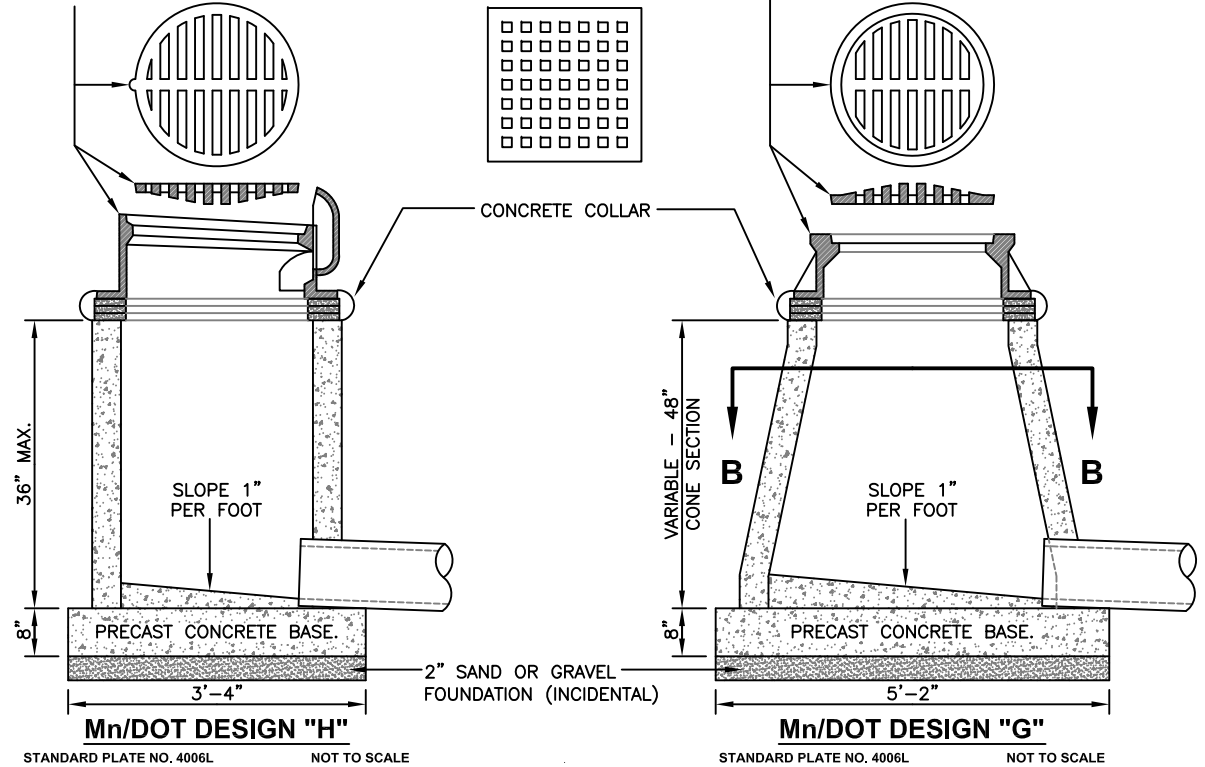
NOTE:

STORM SEWER PIPING SHALL BE PROTECTED DURING EXCAVATION OPERATIONS. IF THE STORM SEWER SYSTEM IS DAMAGED, IT SHALL BE REPAIRED OR REPLACED WITH THE APPROVED TYPE OF PIPING. STORM SEWER REPAIRS TO THE EXISTING PIPING SYSTEM SHALL BE TO THE NEXT JOINT AND SHALL MATCH EXISTING SIZE AND TYPE (RCP SHALL BE EITHER MN/DOT DESIGN 3000 OR 3006 RCP PIPING) UNLESS OTHERWISE APPROVED BY THE ENGINEERING OFFICE. PVC PIPING WITH FERNCO BOOTS WILL NOT BE APPROVED. THE REPLACED PIPING SHALL BE GROUTED INTO THE ADJACENT MANHOLE OR CATCHBASIN INLET.

TYPE "D" CASTING ASSEMBLY
 GRATE FRAME Mn/DOT 801
 GRATE Mn/DOT 810
 CURB BOX Mn/DOT 821

TYPE "C" CASTING ASSEMBLY
 NEENAH CASTING ASSEMBLY
 R3405 OR APPROVED EQUAL

TYPE "B" CASTING ASSEMBLY
 RING Mn/DOT 700-7
 GRATE Mn/DOT 720



MINIMUM STANDARDS FOR STORM SEWER REPAIRS
 NOT TO SCALE